### **NOTES OF IMPORTANCE:**

Submitting for site plan review: sprapplications@cityofmadison.com

Project Status Review: https://www.cityofmadison.com/development-services-center/resources/project-statusinformation

Stormwater Management Permit: https://www.cityofmadison.com/engineering/permits/stormwater-managementpermit

Erosion Control Permit: https://www.cityofmadison.com/engineering/permits/erosion-control-permit

### **SUBMITTING PAYMENT:**

Regular Mail City of Madison Zoning PO Box 2984 Madison, WI 53701-2984

FedEx City of Madison Zoning 215 Martin Luther King Jr. Blvd. Ste. 017 Madison, WI 53703

Dropbox located outside of MMB on East Doty Street.

Make an appointment with the Zoning Counter: https://www.cityofmadison.com/dpced/bi/building-plan-reviewcounter-appointment-/3424/

### **NOTES:**



Department of Planning and Community and Economic Development 215 Martin Luther King Jr Blvd, Suite 017 Madison, WI 53701 266-4551

# Getting Your Parking Lot/Site Plan Approved

In this packet, you'll find:

- Plan Approval Procedures and Instructions
- Parking Lot/Site Plan Approval Application and Checklist
- Example plans for parking lot layout, drainage, and erosion control
- Parking Lot Design Standards  $\bullet$
- Landscape Worksheet
- **Outdoor Lighting Standards**  $\bullet$
- Erosion Control Permit Application
- Storm Water Management Permit Application
- Fire Apparatus Access and Fire Hydrant Worksheet
- Bird-safe glass requirements
- Electric Vehicle

• Application to Excavate in Public Right-of-Way Checklist

## **APPLICATION INSTRUCTIONS – PARKING LOT/SITE PLAN APPROVAL PROCESS**

Our process and forms were designed with input from customers and City staff who provided tips on how to prevent errors and reduce time. Our process works best if we receive complete, accurate and legible information.

Before you submit our plans and required information, discuss your proposal with City staff. At this time you can discuss the City's standards for site design.

<ul> <li>Determine the items you need to include in your application. At a minimum, you need:</li> <li>The Parking Lot/Site Plan Approval Application Checklist</li> <li>PDF of scaled (1" = 20' or similar) drawings</li> <li>To determine the other items you need to include, answer these questions.</li> </ul>
<ul> <li>1. Is this parking lot an approved Conditional Use, Demolition or PD?</li> <li>□ No</li> <li>□ Yes → □ Attach signed copy of Letter of Conditions</li> </ul>
<ul> <li>2. Is a new curb cut being installed?</li> <li>No</li> <li>Yes → □ Apply for street terrace permit at <u>https://elam.cityofmadison.com/CitizenAccess/Default.aspx</u></li> </ul>
<ul> <li>3. Does this site disturb one acre or more?</li> <li>No</li> <li>Yes → □ Submit City of Madison erosion control permit application and approved WDNR NOI permit</li> </ul>
<ul> <li>4. Does this parking lot have a joint driveway or joined parking lots on separate parcels?</li> <li>□ No</li> <li>□ Yes → □ Attach easement agreements</li> </ul>
<ul> <li>5. Will this parking lot have outdoor lighting?</li> <li>□ No</li> <li>□ Yes → □ Attach lighting plan and manufacturers specs &amp; plan</li> </ul>
<ul> <li>6. Does the site meet the applicability standards of Sec. 28.142(2) or is the approved landscape plan changing?</li> <li>No</li> <li>Yes → □ Attach Landscape Worksheet &amp; plan</li> </ul>

Prepare your plans. Use the application checklist to be sure you show all the information that is needed for review of your plans/project.

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**Email checklist, plans, and all attachments to** <u>sprapplications@cityofmadison.com</u>. A zoning staff person will review your application to see if it is complete. If it is not complete, the staff person will contact you and explain what other items you need to submit. If it is complete, a zoning staff person will set up application for review.

**Pay fees.** A site plan review fee is due when plans and attachments are submitted for staff review. The fee is \$100 plus \$50 for each acre of land in excess of one acre, or fraction thereof, up to a maximum of 5 acres or \$300. \$50 fee for Government agencies, schools, NGO's non-profits. You can consult with zoning staff prior to submitting plans to calculate the fee.

**Track site plan review.** You can track the progress of the City agencies' reviews on the <u>City of Madison Site Plan Verification</u> website. If an agency requests additional information, submit the materials or information directly to that agency. If a plan is rejected by an agency, revised plans will need to be resubmitted for a new review along with an additional site plan review fee. You will receive an email notification alerting you when the plans have been approved.

**Obtain appropriate permits.** Once site plan review has been approved by all agencies, you can pay for any permit fees at the Permit Counter and get your permit(s).

# WHO TO CALL FOR HELP

As you look over the application and checklist you may be confused about what you need to submit or show on your plans. City staff can advise you. Give them a call before you draw your plans or submit your application.

ΤΟΡΙϹ	CITY AGENCY	PHONE NUMBER
Building use	Zoning	(608) 266-4551
Setbacks		
Landscaping		
Occupancy		
Parking lot geometrics	Traffic Engineering	(608) 266-4761
Drainage	Engineering	(608) 266-4751
Land disturbing activity		
Soil erosion		
Fire hydrants/access	Fire	(608) 266-4484
Outdoor lighting	Building Inspection	(608) 266-4551

# Why the City Needs So Much Information

The City of Madison reviews and approves parking lot plans to answer these questions:

- Will there be the required number of car/bike spaces for visitors, customers and employees?
- Will drivers of different-sized vehicles be able to get in and out of the spaces safely?
- Will customers and employees with disabilities be able to park and have easy access to the building?
- Can drivers enter and exit the lot safely?
- Will parking lot lighting help keep customers, em property owners?
- Will the lot's construction and use cause minimal soil erosion and runoff?
- Will the lot drain properly?
- Will fire trucks be able to get in and have adequate water supply to put out a fire?
- Will the development meet city aesthetic design requirements?

There is a lot to consider, but understanding why the City reviews parking lot plans will help you understand the information you need to provide for review and approval of your plans.

• Will parking lot lighting help keep customers, employees and property safe, while not disturbing adjacent



# Parking Lot / Site Plan Approval Application Checklist

**Instructions:** Please complete this form and submit it with all the materials necessary for a parking lot plan review and approval. Check boxes for the items submitted that apply to your project. If you are not sure about what to show or submit, call the appropriate agency (*see Box F*). Once your application is accepted, staff will review your application materials within two weeks.

Site Address:		
Contact Person:	Company:	Phone:
Email:		
<b>Project Type</b> (check one): <b>D</b> Permit	ed LNDUSE resubmittal Alteration to PD	□ Alteration to CU □ Alteration to DC/UMX
□ Alteration to TR-P □ Alt	eration to Master Plan	
Summary of Project:		

#### A. These items must be included with an application:

- $\Box$  1. PDF of scaled plans (1" = 20' or similar) emailed to
- sprapplications@cityofmadison.com
- □ 2. Conditional Use or PD/SIP approval letter (*if applicable*)
- □ 3. Easements for joint driveways or joined parking lots on separate parcels (*if applicable*)
- □ 4. Erosion Control Application and Plan (if applicable)
- □ 5. Stormwater Management Permit Application and calculations *(if applicable)*
- □ 6. Landscape Plan/Worksheet (*if applicable per Sec. 28.142(2)*)
- □ 7. Outdoor Lighting Plan and manufacturers specs (*if applicable*)
- 8. Building elevations *(if applicable)*
- □ 9. Building floor plans (*if applicable*)
- □ 10. Bird glass details *(if applicable)*

#### B. Information about your property that must be shown on your drawing(s) (See Example Plans):

- □ 11. Project information block on first page of plan
- □ 12. Property lines
- 13. Abutting right-of-way, roadways, driveways and terraces shown and dimensioned
- □ 14. Elevations of existing and proposed site to City datum
- □ 15. Elevation of top of curb
- □ 16. Storm sewers or drainage pattern (See Example Plan 3)
- 17. Proposed driveway radii
- □ 18. Type of surface on driveway, approach and lot (*grass, landscaping, concrete, bituminous paving, mulch, etc.*)
- □ 19. Location of existing and proposed impervious surfaces
- 20. Means of separation between parking lot and sidewalk or adjoining property
- □ 21. Tree islands
- □ 22. Screening or landscaping (See Example Landscape Plan)
- □ 23. On-site fire hydrants

# C. Information about the structures that must be shown on your drawing (See Example Plans):

- **Q** 24. Existing structures (*footprints and dimensions*)
- □ 25. Proposed structures (footprints and dimensions)
- □ 26. Setbacks and distance to lot lines (*front, rear and sides*)

# D. Parking layout information that must be on your drawing(s) (See Example Plans):

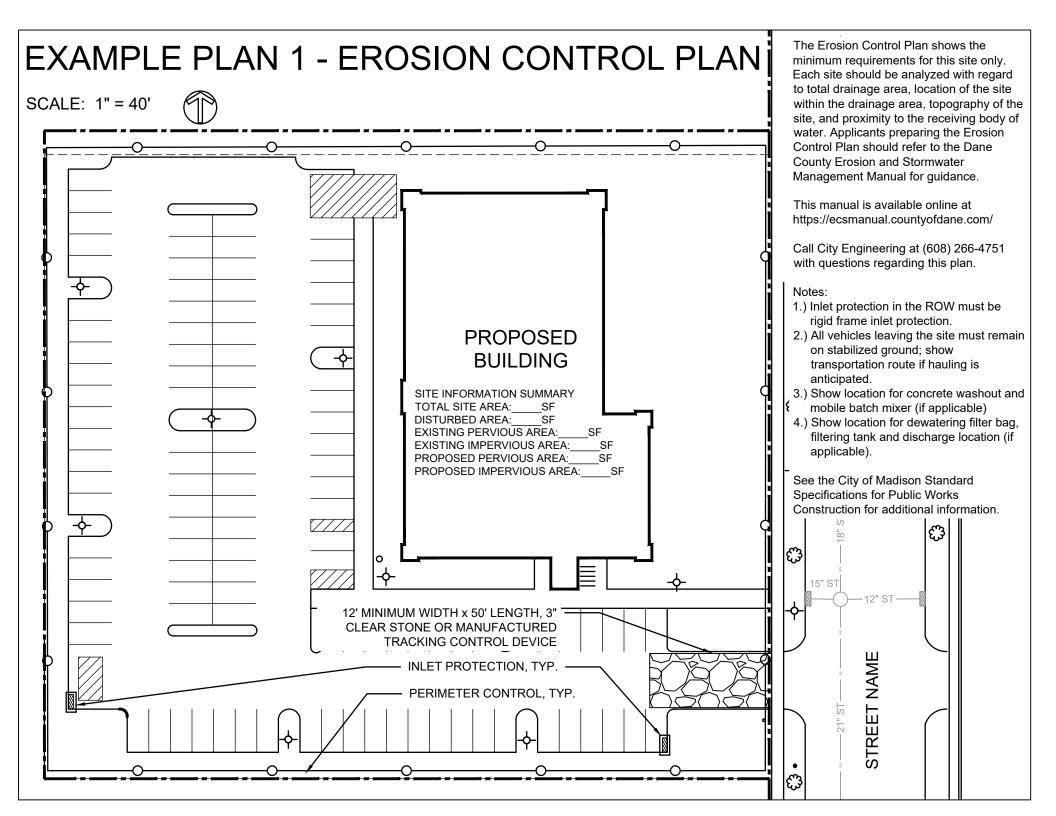
- □ 27. Dimensions of parking stalls and drive aisles
- □ 28. Location of accessible parking stalls
- □ 29. Location of accessible parking stall signs
- □ 30. Location and width of accessibility ramps
- □ 31. Location of loading facilities
- □ 32. Bicycle parking rack locations, spaces, and rack detail
- □ 33. Electric Vehicle charging stalls (*if applicable*)

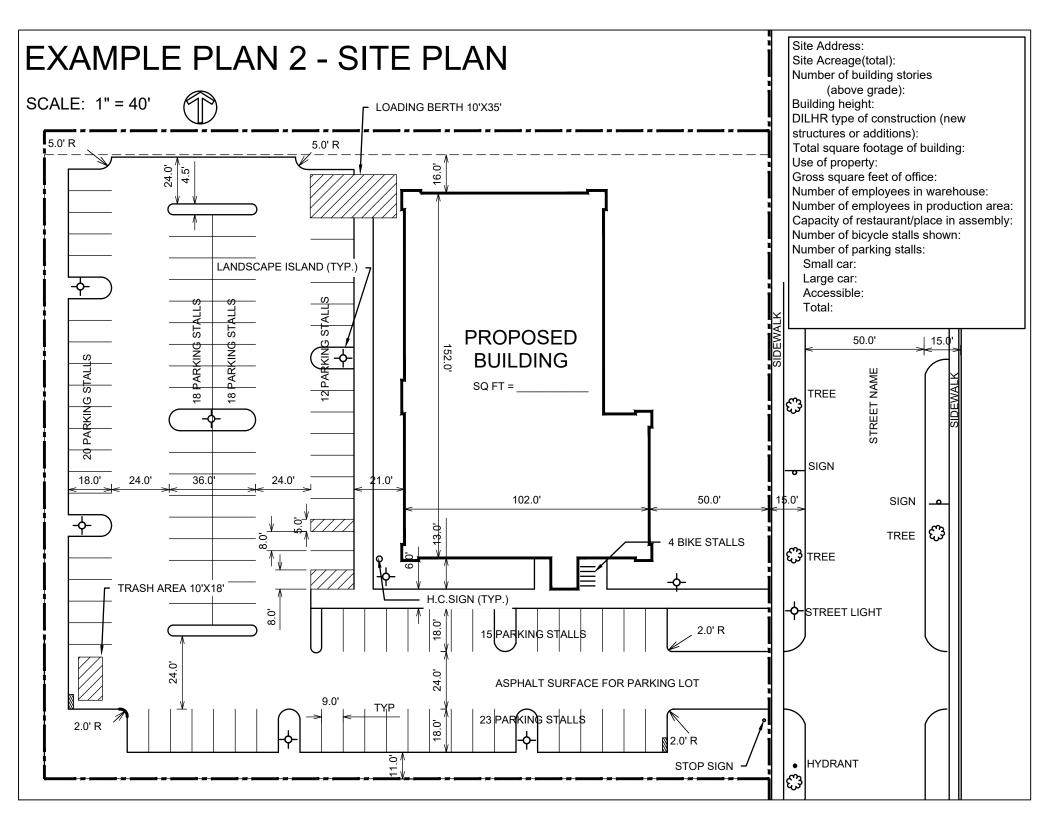
# E. "Off-property" information that must be shown on your drawing(s):

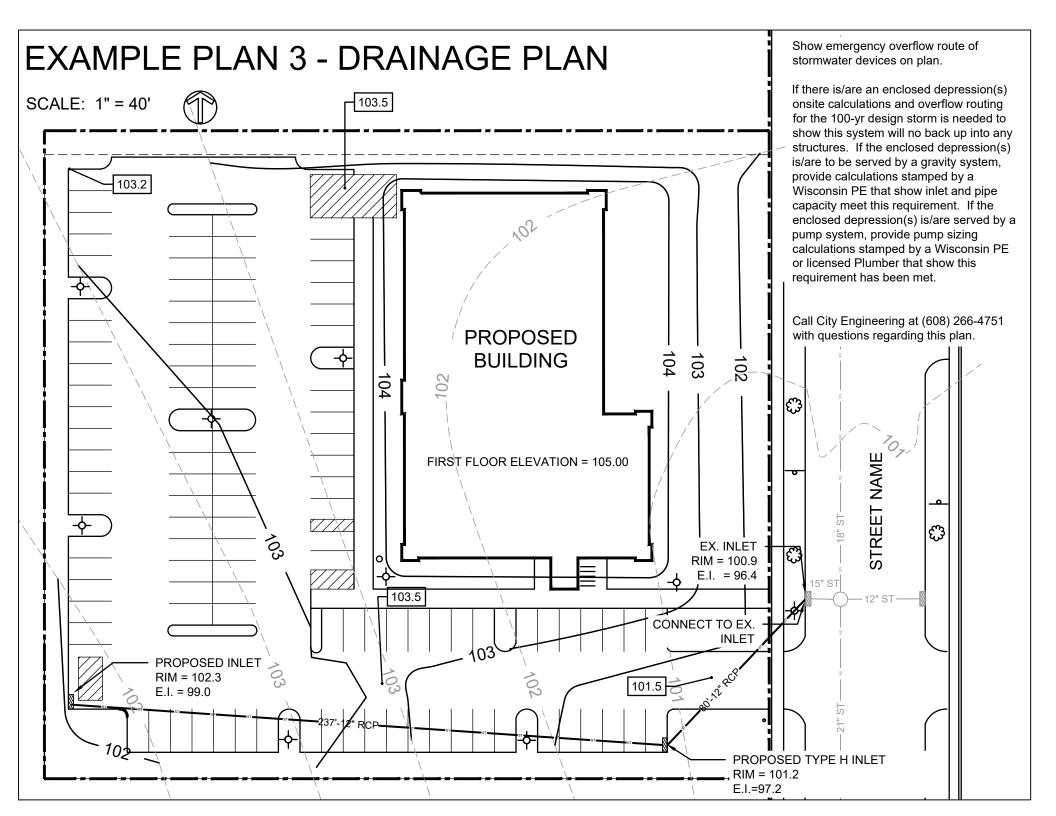
- □ 34. Trees, poles, signs in the right-of-way (*if applicable*)
- □ 35. Medians (*if applicable*)
- □ 36. Driveway openings directly across the street (*if applicable*)
- $\square$  37. Distance to nearest intersection
- □ 38. Fire hydrants within 500 feet of your property line

#### F. Questions: Call City Staff for help.

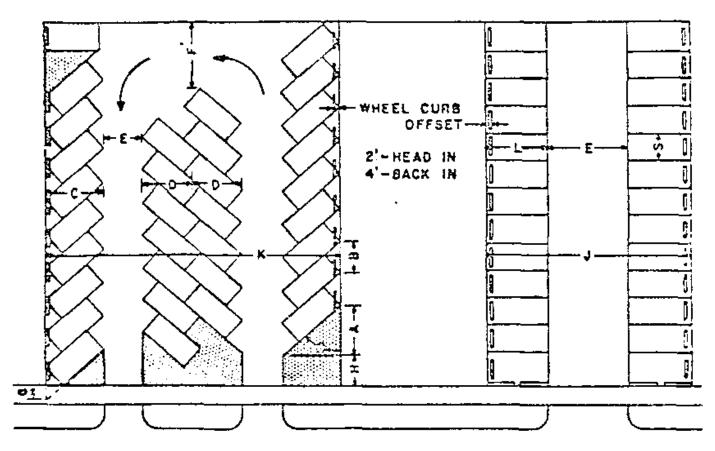
ZONING	Building Use	266-4551
	Setbacks	
	Landscaping	
	Occupancy	
TRAFFIC ENGINEERING	Parking lot geometrics	266-4761
ENGINEERING	Drainage	266-4751
	Land disturbing activity	
	Soil erosion	
Fire	Fire hydrants / access	266-4484
<b>BUILDING INSPECTION</b>	Parking lot lighting	266-4551







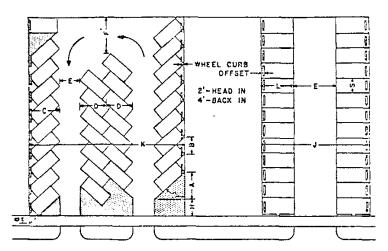
# City of Madison Traffic Engineering Division PARKING DESIGN STANDARDS



### **ONE-SIZE-FITS-ALL**

## TABLE OF DIMENSIONS (IN FEET)

0	S	L	А	В	С	D	E	F	G	Н	J	К
0°	8.75	24.0	0.0	24.0	8.75	8.75	10.5		0.0	0.0	28.0	
20°	8.75	17.0	38.6	25.6	14.1	10.0	9.5		22.6	4.0	37.7	67.2
30°	8.75	17.0	27.9	17.6	16.1	12.3	9.6		13.1	6.0	41.8	76.0
40°	8.75	17.0	21.1	13.7	17.7	14.3	10.0		8.0	8.0	45.4	84.0
45°	8.75	17.0	18.3	12.4	18.3	15.2	10.0	16.0	6.2	9.0	46.6	87.0
50°	8.75	17.0	15.7	11.5	18.7	15.9	10.5	16.5	4.7	10.0	47.9	90.2
60°	8.75	17.0	11.1	10.2	19.1	17.0	17.0	15.0	2.5	12.0	55.2	106.2
70°	8.75	17.0	7.0	9.4	19.0	17.5	18.0	17.0	1.1	14.0	56.0	109.0
80°	8.75	17.0	3.3	8.9	18.3	17.6	23.0	18.0	0.3	16.0	59.6	117.8
90°	8.75	17.0	0.0	8.8	17.0	17.0	23.0	20.0	0.0	0.0	57.0	114.0

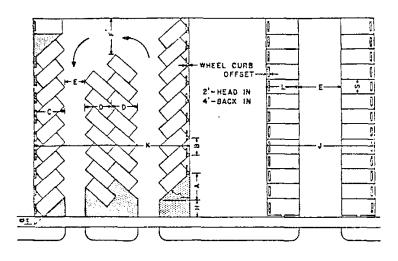


# City of Madison Traffic Engineering Division PARKING DESIGN STANDARDS

### **MEDIUM AND LARGE VEHICLES**

# TABLE OF DIMENSIONS (IN FEET)

0	S	L	Α	В	С	D	E	F	G	н	1	К
0°	8.0	22.0	0.0	22.0	8.0	8.0	11.0		0.0	0.0	27.0	
	8.6	23.0	0.0	23.0	8.5	8.5	10.5		0.0	0.0	27.5	
	9.0	25.0	0.0	25.0	9.0	9.0	10.0		0.0	0.0	28.0	
20°	8.0	18.0	38.5	23.5	14.0	13.0	10.0		19.8	4.0	38.0	74.0
-	8.6	18.0	40.0	25.0	14.5	13.5	9.5		21.8	4.0	38.5	75.0
	9.0	18.0	41.0	26.0	15.0	14.0	9.5		22.8	4.0	39.5	77.0
	9.6	18.0	41.0	28.0	15.0	14.5	9.5		23.8	4.0	39.5	77.0
	10.0	18.0	42.5	29.0	15.5	15.0	9.0		24.8	4.0	40.0	79.0
30°	8.0	18.0	28.5	16.0	15.5	12.5	10.5		12.0	6.0	41.5	77.0
	8.6	18.0	29.0	17.0	16.0	12.5	10.0		12.5	6.0	42.0	77.0
	9.0	18.0	30.0	18.0	16.5	13.0	9.5		13.5	6.0	42.5	78.0
	9.6	18.0	30.0	19.0	17.0	13.5	9.0		14.3	6.0	43.0	79.0
	10.0	18.0	30.5	20.0	17.5	13.5	9.0		14.8	6.0	44.0	80.0
40°	8.0	18.0	21.5	12.5	18.0	14.5	11.0		7.3	9.0	47.0	87.0
	8.6	18.0	22.0	13.0	18.5	14.5	10.5		7.8	8.5	47.5	87.0
	9.0	18.0	22.5	14.0	19.0	15.0	10.0		8.3	8.0	48.0	88.0
	9.6	18.0	22.5	15.0	19.0	15.0	10.0		8.8	7.5	48.0	88.0
	10.0	18.0	23.0	15.5	19.5	15.5	9.5		9.0	7.0	48.5	88.0
45°	8.0	18.0	19.0	11.5	18.0	16.0	11.0	15.0	5.7	10.0	47.0	90.0
	8.6	19.5	19.5	12.0	18.5	16.0	10.5	15.5	6.0	9.5	47.5	90.0
	9.0	18.0	20.0	12.5	19.0	16.0	10.0	16.0	6.4	9.0	48.0	90.0
	9.6	18.0	20.0	13.5	19.5	16.5	9.0	16.5	7.8	8.5	48.0	90.0
	10.0	18.0	20.5	14.0	20.0	16.5	0.0	17.0	7.0	8.0	49.0	90.0
50°	8.0	18.0	16.0	10.5	19.0	16.5	12.0	15.5	4.9	11.0	50.0	95.0
	8.6	18.0	16.5	11.0	19.5	17.0	11.0	16.0	5.0	10.5	50.0	95.0
	9.0	18.0	17.0	12.0	20.0	17.5	10.5	16.5	5.5	10.0	50.5	96.0
	9.6	18.0	17.0	12.5	20.5	18.0	10.0	16.5	5.8	9.5	51.0	97.0
	10.0	18.0	17.0	13.0	20.5	18.0	10.0	17.0	6.0	9.0	51.0	97.0
60°	8.0	18.0	12.0	9.0	20.0	17.5	18.0	15.0	2.3	13.0	58.0	111.0
	8.6	18.0	12.0	10.0	20.5	18.0	17.0	15.0	2.5	12.5	58.0	111.0
	9.0	18.0	12.0	10.5	20.5	18.0	17.0	15.0	2.6	12.0	58.0	111.0
	9.6	18.0	12.5	11.0	21.0	18.0	16.0	15.0	2.8	11.5	58.0	110.0
	10.0	18.0	12.5	11.5	21.0	18.0	15.0	15.0	2.9	11.0	57.0	108.0
70°	8.0	18.0	7.5	8.5	20.5	18.0	19.5	17.0	1.0	15.0	60.5	116.0
	8.6	18.0	7.5	9.0	20.5	18.0	19.0	17.0	1.0	14.5	60.0	115.0
	9.0	18.0	7.5	9.5	20.5	18.0	18.5	17.0	1.0	14.0	59.5	114.0
	9.6	18.0	7.5	10.0	21.0	18.0	17.5	17.0	1.0	13.5	59.5	114.0
	10.0	18.0	7.5	10.5	21.0	18.5	17.0	17.0	1.0	13.0	59.0	113.0
80°	8.0	18.0	3.5	8.0	20.0	17.0	27.0	18.0	0.3	17.5	67.0	128.0
	8.6	18.0	3.5	8.5	20.0	17.5	25.0	18.0	0.3	17.0	65.0	125.0
	9.0	18.0	3.5	9.0	20.0	17.5	23.0	18.0	0.3	16.0	63.0	121.0
	9.6	18.0	3.5	9.5	20.0	17.5	22.0	18.0	0.3	15.5	62.0	119.0
	10.0	18.0	3.5	10.0	20.0	17.5	22.0	18.0	0.3	14.5	62.0	119.0
90°	8.0	18.0	0.0	8.0	18.0	18.0	28.0	20.0	0.0	0.0	64.0	128.0
	8.6	18.0	0.0	8.5	18.0	18.0	26.0	20.0	0.0	0.0	62.0	124.0
	9.0	18.0	0.0	9.0	18.0	18.0	24.0	20.0	0.0	0.0	59.0	120.0
	9.6	18.0	0.0	9.5	18.0	18.0	23.0	20.0	0.0	0.0	59.0	118.0
	10.0	18.0	0.0	10.0	18.0	18.0	22.0	20.0	0.0	0.0	58.0	116.0



# City of Madison Traffic Engineering Division PARKING DESIGN STANDARDS

### **SMALL VEHICLES**

## TABLE OF DIMENSIONS (IN FEET)

0	S	L	Α	В	С	D	E	F	G	н	l	К
0°	7.5	20.0	0.0	20.0	7.5	7.5	9.0		0.0	0.0	24.0	
	8.0	21.0	0.0	21.0	8.0	8.0	8.5		0.0	0.0	24.5	
	8.5	22.0	0.0	22.0	8.5	8.5	8.0		0.0	0.0	25.0	
20°	7.5	16.0	36.0	22.0	12.0	9.0	9.0		18.2	3.5	33.0	60.0
	8.0	16.0	37.5	23.5	12.5	9.0	8.5		19.8	3.5	33.5	60.0
	8.5	16.0	39.0	25.0	13.0	9.5	8.5		21.8	3.5	34.5	62.0
	9.0	16.0	40.5	26.0	13.0	10.0	8.5		22.8	3.5	34.5	63.0
	9.5	16.0	42.0	27.0	13.5	10.5	8.0		23.8	3.5	35.0	64.0
30°	7.5	16.0	25.0	15.0	14.0	11.0	9.5		11.0	5.5	37.5	69.0
	8.0	16.0	26.0	16.0	14.5	11.5	9.0		12.0	5.5	38.0	70.0
	8.5	16.0	27.0	17.0	15.0	12.0	8.5		12.5	5.5	38.5	71.0
	9.0	16.0	28.0	18.0	15.5	12.5	8.0		13.5	5.5	39.0	72.0
	9.5	16.0	28.5	19.0	16.0	13.0	8.0		14.3	5.5	40.0	74.0
40°	7.5	16.0	19.0	11.5	16.0	13.0	10.0		6.8	8.0	42.0	78.0
	8.0	16.0	19.5	12.5	16.0	13.5	10.0		7.3	7.5	42.0	79.0
	8.5	16.0	20.0	13.0	16.5	14.0	9.0		7.8	7.5	42.0	79.0
	9.0	16.0	20.5	14.0	17.0	14.0	8.5		8.3	7.0	42.5	79.0
	9.5	16.0	21.0	15.0	17.0	14.5	8.5		8.8	6.5	42.5	80.0
45°	7.5	16.0	17.0	10.5	16.5	14.0	10.0	13.0	5.2	9.0	43.0	81.0
	8.0	16.0	17.0	11.0	17.0	14.0	9.5	13.5	5.7	8.5	43.5	81.0
	8.5	16.0	17.5	12.0	17.0	14.5	9.5	14.0	6.0	8.0	43.5	82.0
	9.0	16.0	18.0	13.0	17.5	15.0	8.5	14.5	6.4	7.5	43.5	82.0
	9.5	16.0	18.5	13.5	18.0	15.5	8.0	15.0	6.8	7.5	44.0	83.0
50°	7.5	16.0	14.5	10.0	17.0	15.0	11.0	13.5	4.5	10.0	45.0	86.0
	8.0	16.0	15.0	10.5	17.5	15.0	10.5	14.0	4.9	9.5	45.5	86.0
	8.5	16.0	15.0	11.0	18.0	15.5	10.0	14.5	5.0	9.0	46.0	87.0
	9.0	16.0	15.5	12.0	18.0	15.5	10.0	14.5	5.5	8.5	46.0	87.0
	9.5	16.0	16.0	12.5	18.5	16.0	9.0	15.0	4.8	8.0	46.0	87.0
60°	7.5	16.0	10.0	8.5	18.0	15.5	17.0	13.0	2.0	11.5	53.0	101.0
	8.0	16.0	10.5	9.0	18.0	16.0	16.5	13.0	2.3	11.5	52.5	101.0
	8.5	16.0	10.5	10.0	18.0	16.5	16.0	13.0	2.5	11.0	52.0	101.0
	9.0	16.0	11.0	10.5	18.5	16.5	15.5	13.0	2.6	10.5	52.0	101.0
	9.5	16.0	11.0	11.0	18.5	17.0	15.0	13.0	2.8	10.0	52.0	101.0
70°	7.5	16.0	6.5	8.0	18.0	16.0	18.0	15.0	1.0	13.5	54.0	104.0
	8.0	16.0	6.5	8.5	18.0	16.5	17.0	15.0	1.0	13.0	53.0	103.0
	8.5	16.0	6.5	9.0	18.0	16.5	17.0	15.0	1.0	12.5	53.0	103.0
	9.0	16.0	6.5	9.5	18.5	17.0	16.0	15.0	1.0	12.0	53.0	103.0
	9.5	16.0	6.5	10.0	18.5	17.0	16.0	15.0	1.0	11.5	53.0	103.0
80°	7.5	16.0	3.0	7.5	18.0	16.5	24.0	16.0	0.3	15.5	60.0	117.0
	8.0	16.0	3.0	8.0	18.0	16.5	22.0	16.0	0.3	15.5	58.0	113.0
	8.5	16.0	3.0	8.5	18.0	16.5	21.0	16.0	0.3	14.5	57.0	111.0
	9.0	16.0	3.0	9.0	18.0	17.0	20.0	16.0	0.3	14.0	56.0	110.0
	9.5	16.0	3.0	8.5	18.0	17.0	19.0	16.0	0.3	13.0	55.0	108.0
90°	7.5	16.0	0.0	7.5	16.0	16.0	25.0	18.0	0.0	0.0	57.0	114.0
	8.0	16.0	0.0	8.0	16.0	16.0	23.0	18.0	0.0	0.0	55.0	110.0
	8.5	16.0	0.0	8.5	16.0	16.0	22.0	18.0	0.0	0.0	54.0	108.0
	9.0	16.0	0.0	9.0	16.0	16.0	21.0	18.0	0.0	0.0	53.0	106.0
	9.5	16.0	0.0	9.5	16.0	16.0	20.0	18.0	0.0	0.0	52.0	104.0



# CITY OF MADISON LANDSCAPE WORKSHEET

Section 28.142 Madison General Ordinance

Project Location / Address	
Name of Project	
Owner / Contact	
Contact Phone	Contact Email

#### \*\* Landscape plans for zoning lots greater than ten thousand (10,000) square feet in size MUST be prepared by a registered landscape architect. \*\*

#### **Applicability**

The following standards apply to all exterior construction and development activity, including the expansion of existing buildings, structures and parking lots, except the construction of detached single-family and two-family dwellings and their accessory structures. The entire development site must be brought up to compliance with this section unless **all** of the following conditions apply, in which case only the affected areas need to be brought up to compliance:

- (a) The area of site disturbance is less than ten percent (10%) of the entire development site during any ten-(10) year period.
- (b) Gross floor area is only increased by ten percent (10%) during any ten-(10) year period.
- (c) No demolition of a principal building is involved.
- (d) Any displaced landscaping elements must be replaced on the site and shown on a revised landscaping plan.

#### Landscape Calculations and Distribution

Required landscaped areas shall be calculated based upon the total developed area of the property. Developed area is defined as that area within a single contiguous boundary which is made up of structures, parking, driveways and docking/loading facilities, but excluding the area of any building footprint at grade, land designated for open space uses such as athletic fields, and undeveloped land area on the same zoning lot. There are three methods for calculating landscape points depending on the size of the lot and Zoning District.

(a) For all lots except those described in (b) and (c) below, five (5) landscape points shall be provided for each three hundred (300) square feet of developed area.

Total square footage of developed area \_\_\_\_\_

Total landscape points required \_\_\_\_\_

(b) For lots larger than five (5) acres, points shall be provided at five (5) points per three hundred (300) square feet for the first five (5) developed acres, and one (1) point per one hundred (100) square feet for all additional acres.

Total square footage of developed area \_\_\_\_\_

Five (5) acres =  $\underline{217,800}$  square feet

First five (5) developed acres = 3,630 points

Remainder of developed area \_\_\_\_\_

Total landscape points required \_\_\_\_\_

(c) For the Industrial – Limited (IL) and Industrial – General (IG) districts, one (1) point shall be provided per one hundred (100) square feet of developed area.

Total square footage of developed area \_\_\_\_\_

Total landscape points required \_\_\_\_\_\_

#### **Tabulation of Points and Credits**

Use the table to indicate the quantity and points for all existing and proposed landscape elements.

	Minimum Size at	Dist		Existing caping		roposed caping
Plant Type/ Element	Installation	Points	Quantity	Points Achieved	Quantity	Points Achieved
Overstory deciduous tree	2 <sup>1</sup> / <sub>2</sub> inch caliper measured diameter at breast height (dbh)	35				
Tall evergreen tree (i.e. pine, spruce)	5-6 feet tall	35				
Ornamental tree	1 1/2 inch caliper	15				
Upright evergreen shrub (i.e. arborvitae)	3-4 feet tall	10				
Shrub, deciduous	#3 gallon container size, Min. 12"-24"	3				
Shrub, evergreen	#3 gallon container size, Min. 12"-24"	4				
Ornamental grasses/ perennials	#1 gallon container size, Min. 8"-18"	2				
Ornamental/ decorative fencing or wall	n/a	4 per 10 lineal ft.				
Existing significant specimen tree	Minimum size: 2 <sup>1</sup> / <sub>2</sub> inch caliper dbh. *Trees must be within developed area and cannot comprise more than 30% of total required points.	14 per caliper inch dbh. Maximum points per tree: 200				
Landscape furniture for public seating and/or transit connections	* Furniture must be within developed area, publically accessible, and cannot comprise more than 5% of total required points.	5 points per "seat"				
Sub Totals						

#### Total Number of Points Provided \_\_\_\_\_

\* As determined by ANSI, ANLA- American standards for nursery stock. For each size, minimum plant sizes shall conform to the specifications as stated in the current American Standard for Nursery Stock.

# Landscaping shall be distributed throughout the property along street frontages, within parking lot interiors, as foundation plantings, or as general site landscaping. The total number of landscape points provided shall be distributed on the property as follows.

#### Total Developed Area

Required landscaped areas shall be calculated based upon the total developed area of the property. Developed area is defined as that area within a single contiguous boundary which is made up of structures, parking, driveways and docking/loading facilities, but excluding the area of any building footprint at grade, land designated for open space uses such as athletic fields, and undeveloped land area on the same zoning lot.

#### **Development Frontage Landscaping**

Landscaping and/or ornamental fencing shall be provided between buildings or parking areas and the adjacent street(s), except where buildings are placed at the sidewalk. Landscape material shall include a mix of plant materials.

#### **Interior Parking Lot Landscaping**

The purpose of interior parking lot landscaping is to improve the appearance of parking lots, provide shade, and improve stormwater infiltration. All parking lots with twenty (20) or more parking spaces shall be landscaped in accordance with the interior parking lot standards.

#### **Foundation Plantings**

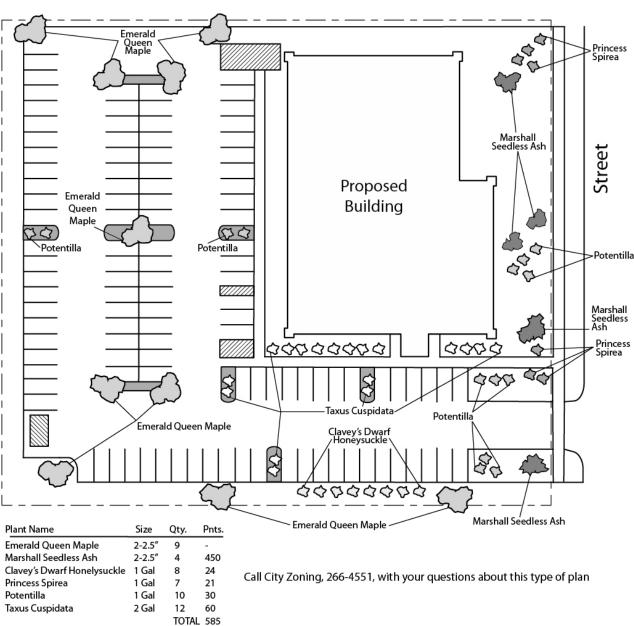
Foundation plantings shall be installed along building facades, except where building facades directly abut the sidewalk, plaza, or other hardscape features. Foundation plantings shall consist primarily of shrubs, perennials, and native grasses.

#### **Screening Along District Boundaries**

Screening shall be provided along side and rear property boundaries between commercial, mixed use or industrial districts and residential districts.

#### **Screening of Other Site Elements**

The following site elements shall be screened in compatibility with the design elements, materials and colors used elsewhere on the site: refuse disposal areas, outdoor storage areas, loading areas, and mechanical equipment.



#### Example Landscape Plan

#### LANDSCAPE PLAN AND LANDSCAPE WORKSHEET INSTRUCTIONS

Refer to Zoning Code Section 28.142 LANDSCAPING AND SCREENING REQUIREMENTS for the complete requirements for preparing and submitting a Landscape Plan and Landscape Worksheet.

#### Applicability.

The following standards apply to all exterior construction and development activity, including the expansion of existing buildings, structures and parking lots, except the construction of detached single-family and two-family dwellings and their accessory structures. The entire development site must be brought up to compliance with this section unless all of the following conditions apply, in which case only the affected areas need to be brought up to compliance:

- (a) The area of site disturbance is less than ten percent (10%) of the entire development site during any ten-(10) year period.
- (b) Gross floor area is only increased by ten percent (10%) during any ten-(10) year period.
- (c) No demolition of a principal building is involved.
- (d) Any displaced landscaping elements must be replaced on the site and shown on a revised landscaping plan.

#### Landscape Plan and Design Standards.

Landscape plans shall be submitted as a component of a site plan, where required, or as a component of applications for other actions, including zoning permits, where applicable. Landscape plans for zoning lots greater than ten thousand (10,000) square feet in size must be prepared by a registered landscape architect.

- (a) Elements of the landscape plan shall include the following:
  - 1. Plant list including common and Latin names, size and root condition (i.e. container or ball & burlap).
  - 2. Site amenities, including bike racks, benches, trash receptacles, etc.
  - 3. Storage areas including trash and loading.
  - 4. Lighting (landscape, pedestrian or parking area).
  - 5. Irrigation.
  - 6. Hard surface materials.
  - 7. Labeling of mulching, edging and curbing.
  - 8. Areas of seeding or sodding.
  - 9. Areas to remain undisturbed and limits of land disturbance.
  - 10. Plants shall be depicted at their size at sixty percent (60%) of growth.
  - 11. Existing trees eight (8) inches or more in diameter.
  - 12. Site grading plan, including stormwater management, if applicable.
- (b) Plant Selection. Plant materials provided in conformance with the provisions of this section shall be nursery quality and tolerant of individual site microclimates.
- (c) Mulch shall consist of shredded bark, chipped wood or stone installed at a minimum depth of two (2) inches. If stone is used, it shall be spread over weed barrier fabric.

#### Landscape Calculations and Distribution.

Required landscaped areas shall be calculated based upon the total developed area of the property. Developed area, for the purpose of this requirement, is defined as that area within a single contiguous boundary which is made up of structures, parking driveways and docking/loading facilities, but **excluding** the area of any building footprint at grade, land designated for open space uses such as athletic fields, and undeveloped land area on the same zoning lot.

- (a) Landscaping shall be distributed throughout the property along street frontages, within parking lot interiors, and as foundation plantings, or as general site landscaping.
- (b) Planting beds or planted areas must have at least seventy-five percent (75%) vegetative cover.
- (c) Canopy tree diversity requirements for new trees:
  - 1. If the development site has fewer than 5 canopy trees, no tree diversity is required.
  - 2. If the development site has between 5 and 50 canopy trees, no single species may comprise more than 33% of trees.
  - 3. If the development site has more than 50 canopy trees, no single species may comprise more than 20% of trees.

#### **Development Frontage Landscaping.**

Landscaping and/or ornamental fencing shall be provided between buildings or parking areas and the adjacent street(s), except where buildings are placed at the sidewalk. Landscape material shall include a mix of plant material meeting the following minimum requirements:

- (a) One (1) overstory deciduous tree and five (5) shrubs shall be planted for each thirty (30) lineal feet of lot frontage. Two (2) ornamental trees or two (2) evergreen trees may be used in place of one (1) overstory deciduous tree.
- (b) In cases where building facades directly abut the sidewalk, required frontage landscaping shall be deducted from the required point total.
- (c) In cases where development frontage landscaping cannot be provided due to site constraints, the zoning administrator may waive the requirement or substitute alternative screening methods for the required landscaping.
- (d) Fencing shall be a minimum of three (3) feet in height, and shall be constructed of metal, masonry, stone or equivalent material. Chain link or temporary fencing is prohibited.

#### **Interior Parking Lot Landscaping.**

The purpose of interior parking lot landscaping is to improve the appearance of parking lots, provide shade, and improve stormwater infiltration. All parking lots with twenty (20) or more parking spaces shall be landscaped in accordance with the following interior parking lot standards.

- (a) For new development on sites previously undeveloped or where all improvements have been removed, a minimum of eight percent (8%) of the asphalt or concrete area of the parking lot shall be devoted to interior planting islands, peninsulas, or landscaped strips. For changes to a developed site, a minimum of five percent (5%) of the asphalt or concrete area shall be interior planting islands, peninsulas, or landscaped strips. A planting island shall be located at least every twelve (12) contiguous stalls with no break or alternatively, landscaped strips at least seven (7) feet wide between parking bays.
- (b) The primary plant materials shall be shade trees with at least one (1) deciduous canopy tree for every one hundred sixty (160) square feet of required landscaped area. Two (2) ornamental deciduous trees may be substituted for one (1) canopy tree, but ornamental trees shall constitute no more than twenty-five percent (25%) of the required trees. No light poles shall be located within the area of sixty percent (60%) of mature growth from the center of any tree.
- (c) Islands may be curbed or may be designed as uncurbed bio-retention areas as part of an approved low impact stormwater management design approved by the Director of Public Works. The ability to maintain these areas over time must be demonstrated. (See Chapter 37, Madison General Ordinances, Erosion and Stormwater Runoff Control.)

#### Foundation Plantings.

Foundation plantings shall be installed along building facades, except where building facades directly abut the sidewalk, plaza, or other hardscape features. Foundation plantings shall consist primarily of shrubs, perennials, and native grasses. The Zoning Administrator may modify this requirement for development existing prior to the effective date of this ordinance, as long as improvements achieve an equivalent or greater level of landscaping for the site.

#### **Screening Along District Boundaries.**

Screening shall be provided along side and rear property boundaries between commercial, mixed use or industrial districts and residential districts. Screening shall consist of a solid wall, solid fence, or hedge with year-round foliage, between six (6) and eight (8) feet in height, except that within the front yard setback area, screening shall not exceed four (4) feet in height. Height of screening shall be measured from natural or approved grade. Berms and retaining walls shall not be used to increase grade relative to screening height.

#### **Screening of Other Site Elements.**

The following site elements shall be screened in compatibility with the design elements, materials and colors used elsewhere on the site, as follows:

- (a) <u>Refuse Disposal Areas.</u> All developments, except single family and two family developments, shall provide a refuse disposal area. Such area shall be screened on four (4) sides (including a gate for access) by a solid, commercial-grade wood fence, wall, or equivalent material with a minimum height of six (6) feet and not greater than seven (7) feet.
- (b) <u>Outdoor Storage Areas.</u> Outdoor storage areas shall be screened from abutting residential uses with a by a building wall or solid, commercial-grade wood fence, wall, year-round hedge, or equivalent material, with a minimum height of six (6) feet and not greater than seven (7) feet. Screening along district boundaries, where present, may provide all or part of the required screening.
- (c) <u>Loading Areas.</u> Loading areas shall be screened from abutting residential uses and from street view to the extent feasible by a building wall or solid, commercial-grade wood fence, or equivalent material, with a minimum height of six (6) feet and not greater than seven (7) feet. Screening along district boundaries, where present, may provide all or part of the required screening.
- (d) <u>Mechanical Equipment.</u> All rooftop and ground level mechanical equipment and utilities shall be fully screened from view from any street or residential district, as viewed from six (6) feet above ground level. Screening may consist of a building wall or fence and/or landscaping as approved by the Zoning Administrator.

#### Maintenance.

The owner of the premises is responsible for the watering, maintenance, repair and replacement of all landscaping, fences, and other landscape architectural features on the site. All planting beds shall be kept weed free. Plant material that has died shall be replaced no later than the upcoming June 1.

# CITY OF MADISON OUTDOOR LIGHTING STANDARDS



#### **Approval Process**

Submit the following to the Zoning Counter in the Inspection Unit:

- 1. A catalog page, cut sheet, or photograph of the lighting fixtures, including the mounting method with a graphic depiction of the lamp concealment and light cutoff angles of the lighting fixture.
- 2. A photometric data report of the proposed lighting fixture graphically showing the distribution in all angles vertically and horizontally around the fixture (this is available from your lighting supplier).
- 3. A plot plan showing:
  - a. The location of all outdoor lighting fixtures proposed,
  - b. The mounting of installation height,
  - c. The overall illumination levels and uniformities,
  - d. The point where 0.5 horizontal footcandles occurs on the property or adjacent property at a distance four (4) feet above the ground.

This may be accomplished by means of an isolux curves or a photometric plot of the illumination levels.

#### Design Requirements – Open Parking Facilities

The illumination requirements of an open parking facility depend on the amount of usage the facility receives. Three levels of activity shall be established as high, medium, and low, reflecting both traffic and pedestrian activity. The following examples are nonexclusive and include:

High Activity (.12): Facilities for major league athletic events or major cultural or civic events.

**Medium Activity (.10):** Shopping centers, retail parking areas, hospital and clinic parking areas, transportation parking (airports, commuter lots, etc.), cultural, civic or recreational events, and fast food facilities.

Low Activity (.08): Employee parking, educational facility parking, office parks, and church parking.

- An outdoor lighting system for illuminating buildings and structures shall have a maximum connected lighting load of five (5) watts per lineal foot. Watts shall mean lamp wattage and ballast consumption.
- A residential site shall be lighted to provide at least .25 footcandles on any surface in the lot with an average illumination level of at least .75 footcandles. Outdoor light fixtures shall be designed and installed to minimize light trespass. In addition, the uniformity ration between the average illumination and minimum illumination shall be no greater than 4:1.
- For an outdoor merchandising area, the maximum level of 75% in the lot shall not exceed 20 footcandles. A contiguous area not to exceed 25% of the lot may be illuminated to a level which shall not exceed 40 footcandles.
- The maximum illumination level under an outdoor canopy shall not exceed 50 footcandles at any point.

#### HORIZONTAL ILLUMINANCES FOR PARKING FACILITIES

Level of Activity	Min. Footcandles on Pavement <sup>1</sup>	Max. Avg. Footcandles on Pavement	Max. Uniformity Ratio <sup>1</sup> (Avg:Min)	Max Watts <sup>2</sup> /Sq. Ft. Lighting Load <sup>3</sup>	Min. Footcandles on Pavement <sup>1</sup>	Max. Avg. Footcandles on Pavement	Max Uniformity Ratio <sup>1</sup> (Avg:Min)
High	0.6 fc	3.75 fc	5:1	.12	.67 fc	2.5 fc	5:1
Med	0.4 fc	2.50 fc	5:1	.10	.33 fc	1.5 fc	5:1
Low	0.2 fc	1.50 fc	5:1	.08	.125 fc	1.0 fc	5:1

#### a. Open Parking Facilities

#### b. Covered Parking Facilities

Areas	Minimum Footcandle Average on Pavement	Minimum Footcandles on Pavement	Maximum Average Footcandles on Pavement	Maximum Uniformity Ratio (Avg:Min)	Maximum Watts/Sq. Ft. Lighting Load
General parking & ped. areas	5 fc	1.25 fc	9 fc	4:1	.2
Private controlled entry parking	3 fc	.75 fc	6 fc	4:1	.2

#### > The International Code Council, National Electrical Code, and others may have additional requirements.

> For further information, call Building Inspection at 266-4568.

 <sup>&</sup>lt;sup>1</sup> Not mandatory within 4 feet of the pavement edge.
 <sup>2</sup> Not mandatory for driveways
 <sup>3</sup> Watts shall mean lap wattage and ballast consumption.



# Application for Erosion Control Permit

### City of Madison Engineering Division

City-County Building, Room 115 • 210 Martin Luther King, Jr. Blvd. • Madison, WI 53703

#### **SECTION 1 – PROPERTY INFORMATION**

Project Name:	
Property Address:	Street (or Parcel Number)
	City, State Zip
	Plat or CSM
	East of the Capital West of the Capital
	SECTION 2 – LANDOWNER INFORMATION
Company:	
Full Name:	
	Last, First
Mailing Address:	Street
	City, State Zip
Contact Phone:	Email:
SE	ECTION 3 – APPLICANT INFORMATION (if different than LANDOWNER)
	e responsible for compliance with MGO Chapter 37 and the conditions of the permit, or violations that occur on the premises.
Same as LANDO	WNER (check if YES, and continue with Section 4)
Company:	
Full Name:	
	Last, First
Mailing Address:	Street
	City, State Zip
Contact Phone:	Email: (REQUIRED)

#### SECTION 4 – AUTHORIZED EROSION CONTROL INSPECTOR (IF KNOWN)

Same as APPLICAN	IT (check if YES, and continue	with Section 5)			
Company:					
Full Name:	Last, First				
Mailing Address:					
	City, State Zip				
Contact Phone:		Email: REQUIRED)			
	SECTION 5 – ENGINEE	R INFORMATION (OP	TIONAL)		
Engineer Name:					
Contact Phone:	E	Email:			
	SECTION 6	<b>5 – PERMIT TYPE</b>			
Check only one option.					
<ol> <li>Provide cor</li> <li>Protect any</li> <li>Provide per</li> <li>Provide tim</li> <li>Full Erosion Contro</li> <li>Attach eros</li> </ol>	cklist – Disturbed Area<20,000 istruction entrances with tracki inlets receiving run-off from the imeter control to retain sedime ely restoration with 14 days of I Plan (\$200.00 permit base fe ion control report and plan for	ing controls. The disturbed construction ents on the construction land disturbance activi e) review.	on area. 1 site. ties.		
See attache	ed Standard Operating Proced	ures for enforcement p	rotocols.		
Renewal of Expired	Permit (\$50.00 permit base fe	ee)			
	SECTION 7 –	FEE CALCULATION			
Permit Base Fee (Permit type selected a	above in Section 6)	\$	FEES RECEIVED Office Use Only		
Total Disturbed Area	(ft <sup>2</sup> )	ft²	Date		
Erosion Control Area Plan ONLY (\$5/1000 f	t <b>Fee</b> - Full Erosion Control t <sup>2</sup> disturbed)	\$	Amt		
	Total Fees	\$	ву		
Make checks payable to	o "City Treasurer."				
USLE Rate (ton/acre/ye	ear): Construc	tion Start Date:	Restoration Date:		
			or borrow pits shall be allowed in park y proceed until this application has been		

area without approval by the Parks Division prior to construction. NO land disturbance work may proceed until this application has been approved and a permit issued. If the disturbed area is larger than 1 acre, permit will not be issued until copy of approved WDNR permit is received.

#### **SECTION 8 – LANDOWNER AND APPLICANT SIGNATURE**

I have reviewed and understand Chapter 37 of the Madison General Ordinances regarding erosion control, and I shall implement the control plan or checklist for this project as approved by the City.

As a condition of the granting of this permit, I authorize, and have the authority to authorize, City of Madison personnel the right-of-entry onto the above described premises for the purpose of inspecting and monitoring for compliance with the aforesaid ordinance.

Projects located in the Rock River TMDL Zone are subject to increased erosion control standards and enforcement in accordance with Resolution 14-00043 passed by the Madison Common Council on 1/21/2014 and will be expected to meet a higher standard of erosion control than the minimum standards set by the WDNR.

I acknowledge by submitting this application and signing below, that I shall be responsible for compliance with MGO Chapter 37 and the conditions of this permit.

LANDOWNER SIGNATURE	DATE	
LANDOWNER NAME (PLEASE PRINT)	-	
APPLICANT SIGNATURE	DATE	
APPLICANT NAME (PLEASE PRINT)	-	



# **Stormwater Management Permit Application**

City of Madison Engineering Division

210 Martin Luther King Jr. Blvd. D City-County Building Suite 115 D Madison, WI 53703

Ducia at Nor		ion 1 <b>Property Information</b>	
Project Nan	ne:		
Property			
Address:	Street	Lot Number(s)	Parcel Number
	City	State	ZIP Code
	Plat or CSM		
	Sect	tion 2 🛛 Landowner Information	
Full Name:	Last	First	M.I.
Mailing Address:		7 // 50	141.1.
	Street		Apartment/Unit #
	City	State	ZIP Code
Contact Ph	ione:	E-Mail:	
	er than landowner requires a notanzed	statement authorizing the applicant to act as the landowner's ag	gent. Form must be attache
Full Name:		statement authorizing the applicant to act as the landowner's ac adowner (Check if YES, and continue with Section 4)	gent. Form must be attache
Full Name:			gent. Form must be attache M.I.
Full Name: Mailing Address:	Same as Lar	ndowner (Check if YES, and continue with Section 4)	
Mailing	Same as Lar	ndowner (Check if YES, and continue with Section 4)	
Mailing	Last	ndowner (Check if YES, and continue with Section 4)	М.І.
Mailing	City Same as Lar	ndowner (Check if YES, and continue with Section 4) First	M.I. Apartment/Unit #
Mailing Address:	Same as Lar  Last  Street  City  One:	ndowner (Check if YES, and continue with Section 4) First State	M.I. Apartment/Unit #
Mailing Address:	Same as Lar  Last  Street  City  One:	ndowner (Check if YES, and continue with Section 4) First State E-Mail:	M.I. Apartment/Unit #
Mailing Address:	Same as Lar  Last  Street  City  One:	Adowner (Check if YES, and continue with Section 4) First State E-Mail: ection 4   Site Information Total Site Area Existing Impervious Area	M.I. Apartment/Unit # ZIP Code
Address:	Same as Lar      Last      City  one:      Solution      Solution	Indowner (Check if YES, and continue with Section 4)         First         State         E-Mail:         Image: State Information         Total Site Area         Image: Before Project)         New Impervious Area         (Before Project)         New Impervious Area	M.I. Apartment/Unit # ZIP Code ft <sup>2</sup>
Mailing Address:	Same as Lar      Last      City  one:      (Impervi  )      (Impervi )	Indowner (Check if YES, and continue with Section 4)         First         State	M.I. Apartment/Unit # ZIP Code ft <sup>2</sup> ft <sup>2</sup>
Mailing Address:	Same as Lar      Last      City  one:      (Impervi  )  (Impervi )  )  )  (Impervi )  )  )  )  )  )  )  )  )  )  )  )  )	hdowner (Check if YES, and continue with Section 4)  First  State  E-Mail:  Contemporation  Total Site Area  (Before Project)  New Impervious Area (Before Project)  New Impervious Area (Before Project)  New Impervious Area (Before Project)  State  State State State Sta	M.I. Apartment/Unit # ZIP Code ft <sup>2</sup> ft <sup>2</sup> ft <sup>2</sup>

Work to be	performed by	(if known):
------------	--------------	-------------

Same as Applicant (Check if YES)

Contact Phone:		E-Mail:		
**Plea		ement Report/Plan to be ot be processed without r		
	Section 5 🛛	Fee Calculation		
	e information from Section			
Permit Base Fee	$(f_{1}, 0, (1, 0, 0, 0), H^2)$	\$ 400.00	Dete	Office Use Only
<ul><li>(A) New Impervious Area Fee (</li><li>(B) Redeveloped Impervious Ar</li></ul>		\$ \$		
		Ψ	- Ann -	
	Total Fees	\$	Ву	
Sec TSS Reduction: Oil & Grease Removal Runoff Rate Control/Dete Infiltration Groundwater Recharge Thermal Control Maintenance Agreement	ention 6 <b>a</b> Stormwat	<b>er Management Rec</b> : (80%)	uirement	ts

#### Section 7 Applicant Signature

I have reviewed and understand Chapter 37 of the Madison General Ordinances regarding erosion control, and I shall implement the control plan or checklist for this project as approved by the city.

I further, in accordance with Chapter 37, grant the right-of-entry onto this property, as described above, to the designated personnel of the City of Madison for the purpose of inspecting and monitoring for compliance with the aforesaid ordinance.

Applicant Signature \_\_\_\_\_ Date of Application \_\_\_\_\_

\*Applicant other than landowner requires a notarized statement authorizing the applicant to act as the landowner's agent—must be attached

#### APPLICATION TO EXCAVATE IN PUBLIC RIGHT-OF-WAY SUBMITTAL CHECKLIST

#### Submittal Includes:

Back of curb, right-of-way, property lines, and existing City utilities (water, sanitary sewer, storm sewer, and traffic engineering) displayed on site plan(s).
 The City of Madison Engineering Dept. posts utility data online for viewing

through a web-based program called GTWeb. To register, please contact: <u>gtweb@cityofmadison.com</u>

- Dimensions provided off of the existing features listed above to proposed utility placement.
- Traffic Control Plan (including pedestrian & bicycle designs as well as any temporary measures for potholing where applicable,).
   Submissions may be able to reference individual pages from the Work Zone Safety Handbook that correlate with site conditions: http://epdfiles.engr.wisc.edu/pdf\_web\_files/tic/handbooks/WorkZoneSafety.pdf
- Expected areas of ROW occupancy for installation equipment shown (work zones).
- All contractors and subcontractors performing work listed on application.
   <u>All contractors & subcontractors must be pre-qualified with the City of Madison</u>
- $\Box \qquad \text{Copy of notification letter mailed to property owners and list of property owners notified for projects extending more than one block in residential or commercial areas.}$  $<math display="block">\Box \mathbf{N}/\mathbf{A} \text{ (project is less than 1 block in length)}$

#### APPLICATIONS THAT DO NOT MEET THESE CRITERIA WILL BE REJECTED.

	Horizontal Clearance (ft)	Vertical Clearance (ft)
Back of Curb	6	-
Sanitary Sewer (mains, laterals and structures)	3	2
<b>Storm Sewer</b> (mains, laterals, and structures)	3	2
<b>Traffic Engineering</b> (conduit and structures)	3	3
Water Utility (mains, valves, services, and hydrants)	4	3

Clearance requirements for design of proposed utility installations:

Submittal instructions: Include a pdf copy of this form at the front of your SWM report. A copy of the excel file shall also be submitted available at:

Project Name	https://www.cityofmadison.com/engineering/permits/stormwater-management-permit			
Street Address				
Parcel Number	All boxes shaded green should be populated.			
Project Description (1-3	For sites with multiple basins, multiple copies of the Peak Runoff Rates and Volumes table r	nay be needed.		
sentences):				

Site Impervious Area Summary	Square Feet	Acres
Total Site Area		-
Existing Impervious Area		-
New Impervious Area		-
Redeveloped Impervious Area		-
Removed Impervious Area		-
Net Impervious Area	-	-

Parcel Area Summary	Square Feet	Acres
Total Parcel Area		0.00
Total Site Area Draining to SWM		
BMP(s)		0.00
Total Offsite Area Draining to		
SWM BMP(s)		0.00
Total Uncontrolled Area		0.00

TSS Removal					
Number of lbs	Number of lbs				
(without controls)	(with controls)	% Reduction			
		#DIV/0!			

Phosphorus Removal					
Number of lbs (without controls)	Number of lbs (with controls)	% Reduction	Does BMP have an underdrain? (Y/N)		
		#DIV/0!			

	Predevelopment		Post Dev	velopment Re		tion %	Peak Water
	Rate	Volume	Rate	Volume	Rate	Volume	Elevation in
Peak Runoff Rates and Volumes	(cfs)	(ac-ft)	(cfs)	(ac-ft)	(%)	(%)	Basin (ft)
1-Year, 24-Hour Storm (2.49")					#DIV/0!	#DIV/0!	
2-Year, 24-Hour Storm (2.84")					#DIV/0!	#DIV/0!	
5-Year, 24-Hour Storm (3.51")					#DIV/0!	#DIV/0!	
10-Year, 24-Hour Storm (4.09")					#DIV/0!	#DIV/0!	
100-Year, 24-Hour Storm (6.66")					#DIV/0!	#DIV/0!	
200-Year, 24-Hour Storm (7.53")					#DIV/0!	#DIV/0!	
500-Year, 24-Hour Storm (8.94")					#DIV/0!	#DIV/0!	

Redevelopment Metrics (if applicable)	Reducti	Reduction Provided			
Does proposed impervious area exceed 80% of existing site impervious area? (If NO,					
you can skip to the next section)	NO	#[	DIV/0!		
Peak Runoff Rates from the site reduced by 15% during a 10-year storm	N/A	#[	DIV/0!		
Reduce runoff volume by 5% from the site during 10-year storm	N/A	#[	DIV/0!		
Minimum Required Storage for First 1/2" of Rainfall over New + Redeveloped					
Impervious Area	-	cf		-	ac-ft
Storage Provided by Green Infrastructure		cf			
Green infrastructure provides detention for first 1/2" of rainfall	N/A		_		

	Infiltration		
			90%
Predevelopment Stay On	<b>Required Stay</b>	Provided Stay	Infiltration
(inches)	On (inches)	On (inches)	Achieved
			NO

Oil and Grea		
Oil and Grease Control Require	red? (Y/N)	
BMP	Quantity	
Bioretention Basin		
Inlet filters		
Snout System		
Rock Trench		

Thermal Control (Sugar River Watershed only)						
Thermal Control Required	d? (Y/N)					
BMP	Quantity					
Bioretention Basin						
Infiltration Basin						
Rock Trench						
Other						

Flooding Checks (if applicable)		
Lowest opening elevation of proposed development		ft
Confirm that overflow from pond does not enter any structures.	Not Applicable	
	<b>C C</b>	

If site has underground parking and trench drain, confirm there's a safe overflow route/sump pump is sized for the 100-year storm. Not Applicable



# **City of Madison Fire Department**

30 West Mifflin Street, 8<sup>th</sup> & 9<sup>th</sup> Floors, Madison, WI 53703-2579 Phone: 608-266-4420 • Fax: 608-267-1100 • E-mail: fire@cityofmadison.com

**Project Address:** 

Contact Name & Phone #:

#### FIRE APPARATUS ACCESS AND FIRE HYDRANT WORKSHEET

<ol> <li>Is the building completely protected by an NFPA 13 or 13R automatic fire sprinkler system? If non-sprinklered, fire lanes extend to within 150-feet of all portions of the exterior wall? If sprinklered, fire lanes are within 250-feet of all portions of the exterior wall?</li> </ol>	☐ Yes ☐ Yes ☐ Yes	☐ No ☐ No ☐ No	□ N/A □ N/A □ N/A
<ul> <li>2. Is the fire lane constructed of concrete or asphalt, designed to support a minimum load of 85,000 lbs?</li> <li>a) Is the fire lane a minimum unobstructed width of at least 20-feet?</li> <li>b) Is the fire lane unobstructed with a vertical clearance of at least 13<sup>1</sup>/<sub>2</sub>-feet?</li> <li>c) Is the minimum inside turning radius of the fire lane at least 28-feet?</li> <li>d) Is the grade of the fire lane not more than a slope of 8%?</li> <li>e) Is the fire lane posted as fire lane? (Provide detail of signage.)</li> <li>f) Is a roll-able curb used as part of the fire lane? (Provide detail of curb.)</li> <li>g) Is part of a sidewalk used as part of the required fire lane? (Must support +85,000 lbs.)</li> </ul>	<ul> <li>Yes</li> <li>Yes</li> <li>Yes</li> <li>Yes</li> <li>Yes</li> <li>Yes</li> <li>Yes</li> <li>Yes</li> <li>Yes</li> </ul>	No     No	□ N/A □ N/A □ N/A □ N/A □ N/A □ N/A □ N/A
<ul><li>3. Is the fire lane obstructed by security gates or barricades? If yes:</li><li>a) Is the gate a minimum of 20-feet clear opening?</li><li>b) Is an approved means of emergency operations installed, key vault, padlock or key switch?</li></ul>	☐ Yes ☐ Yes ☐ Yes	☐ No ☐ No ☐ No	□ N/A □ N/A □ N/A
4. Is the Fire lane dead-ended with a length greater than 150-feet? If yes, does the area for turning around fire apparatus comply with IFC D103?	Yes Yes	No No	□ N/A □ N/A
5. Is any portion of the building to be used for high-piled storage in accordance with IFC Chapter 3206.6 If yes, see IFC 3206.6 for further requirements.	Yes	🗌 No	N/A
6. Is any part of the building greater than 30-feet above the grade plane?	Yes	🗌 No	N/A
<ul> <li>If yes, answer the following questions:</li> <li>a) Is the aerial apparatus fire lane parallel to one entire side of the building and covering at least 25% of the perimeter?</li> <li>b) Is the near edge of the aerial apparatus fire lane between 15' and 30' from the building?</li> <li>c) Are there any overhead power or utility lines located across the aerial apparatus fire lane?</li> <li>d) Are there any tree canopies expected to grow across the aerial fire lane? (Based on mature</li> </ul>	☐ Yes ☐ Yes ☐ Yes ☐ Yes ☐ Yes	<ul> <li>No</li> <li>No</li> <li>No</li> <li>No</li> <li>No</li> </ul>	□ N/A □ N/A □ N/A □ N/A □ N/A
<ul> <li>If yes, answer the following questions:</li> <li>a) Is the aerial apparatus fire lane parallel to one entire side of the building and covering at least 25% of the perimeter?</li> <li>b) Is the near edge of the aerial apparatus fire lane between 15' and 30' from the building?</li> <li>c) Are there any overhead power or utility lines located across the aerial apparatus fire lane?</li> </ul>	☐ Yes ☐ Yes ☐ Yes	<ul> <li>□ No</li> <li>□ No</li> <li>□ No</li> </ul>	□ N/A □ N/A □ N/A
<ul> <li>If yes, answer the following questions:</li> <li>a) Is the aerial apparatus fire lane parallel to one entire side of the building and covering at least 25% of the perimeter?</li> <li>b) Is the near edge of the aerial apparatus fire lane between 15' and 30' from the building?</li> <li>c) Are there any overhead power or utility lines located across the aerial apparatus fire lane?</li> <li>d) Are there any tree canopies expected to grow across the aerial fire lane? (Based on mature canopy width of tree species)</li> <li>e) Does the aerial apparatus fire lane have a minimum unobstructed width of 26-feet?</li> </ul>	<ul> <li>☐ Yes</li> <li>☐ Yes</li> <li>☐ Yes</li> <li>☐ Yes</li> <li>☐ Yes</li> </ul>	□ No □ No □ No □ No □ No	<ul> <li>□ N/A</li> <li>□ N/A</li> <li>□ N/A</li> <li>□ N/A</li> <li>□ N/A</li> </ul>

Attach an additional sheet if further explanation is required for any answers.

This worksheet is based on MGO 34.503 and IFC 2012 Edition Chapter 5 and Appendix D; please see the codes for further information.



#### Department of Planning & Community & Economic Development Building Inspection Division

Madison Municipal Building, Suite 017 215 Martin Luther King Jr. Blvd. P.O. Box 2984 Madison, Wisconsin 53701-2984 Phone: (608) 266-4551 Fax (608) 266-6377 www.cityofmadison.com

Date:	September 1 2020
From:	Matt Tucker, Zoning Administrator
Subject:	New Bird-Safe Glass Requirement

On 8/4/2020 the Madison Common Council approved new regulations requiring bird-safe glass in certain situations. These regulations apply to "all exterior construction and development activity, including the expansion of existing buildings and structures..." on all buildings with over 10, 000 sq. ft. of floor area above grade. This means that all new buildings, new window openings in existing buildings, and building additions will require bird-safe glass, when applicable. Replacement windows in existing openings will not require bird-safe treatment. The ordinance takes effect on October 1<sup>st</sup>, 2020 in order to give developers, architects and planners time to learn about the new regulations and incorporate the requirements in future projects.

Bird-safe glass is required when buildings meet either of two criteria:

- 1. Building façades where the first sixty (60) feet from grade are comprised of greater than or equal to fifty (50%) glass,
- 2. Buildings not meeting (1) above, but where the building has glass windows that are greater than 50 square feet in area in the first 60 feet from grade or glass on both sides of a corner within 15' of a corner.

In cases where bird-safe glass is required, the following features satisfy the requirement:

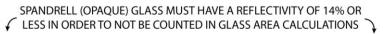
- 1. Dots or other shapes that are ¼" in diameter or larger and spaced at no more than a 2" by 2" pattern.
- 2. Lines that are 1/8" in width or greater and spaced no more than 2" apart.
- 3. Low reflectance opaque materials.
- 4. Building-integrated structures like non-glass double-skin facades.
- 5. Metal screens.
- 6. Fixed solar shading.
- 7. Exterior insect screens.
- 8. Other similar mitigation treatments approved by the Zoning Administrator.

Additionally, bird-safe glass is always required for any at-grade glass feature, glass railings, or glass connections between buildings such as skyways or breezeways.

The text and images of the new ordinance are found on the following pages.

#### 28.129 BIRD-SAFE GLASS REQUIREMENTS.

- (1) <u>Statement of Purpose</u>. The Bird-Safe Glass Requirements in this section are intended to reduce the heightened risk for bird collisions with glass on specified building designs and configurations.
- (2) <u>Applicability</u>. Subsection (4) applies to all exterior construction and development activity, including the expansion of existing buildings and structures, as specified therein.
- (3) <u>Measuring Glass Area</u>. Under this ordinance, glass area shall be measured as one (1) continuous panel of glass or other transparent material, or a set of two (2) or more such panels divided by mullions of six (6) inches in width or narrower. Panels surrounded on all sides by solid walls or mullions wider than six (6) inches shall be considered individual windows. Spandrel or opaque glass with reflectivity of 14% or less shall not be included in the calculation of glass area. (See Figure 1)
- (4) <u>Bird-Safe Glass Treatment Requirements</u>. Glass areas on the following buildings or structures shall be treated to reduce the risk of bird collisions by incorporating a pattern of visual markers that are either: a) dots or other isolated shapes that are ¼" in diameter or larger and spaced at no more than a two-inch (2") by two-inch (2") pattern; or b) lines that are 1/8" in width or greater and spaced no more than 2" apart; low reflectance opaque materials; building-integrated structures like non-glass double-skin facades, metal screens, fixed solar shading, exterior insect screens, and other features that cover the glass surface; or other similar mitigation treatments approved by the Zoning Administrator.
  - (a) <u>Buildings or structures over 10,000 square feet</u>. For any building or structure over 10,000 square feet in size (floor area of above-grade stories), bird-safe glass treatment is required as follows:
    - 1. For building façades where the first sixty (60) feet <u>(see Figure 2)</u> from grade are comprised of greater than or equal to fifty (50%) glass:
      - a. At least eighty-five percent (85%) of the glass must be treated; and
      - b. All glass within fifteen (15) feet of a building corner must be treated when see through or fly through conditions exist. (See Figure 3.)
    - 2. For building façades where the first sixty (60) feet from grade are comprised of less than fifty percent (50%) glass:
      - At least eighty-five percent (85%) of the glass on-glass areas fifty
         (50) square feet or over must be treated; and
      - b. Of all glass areas over fifty (50) square feet, any glass within fifteen (15) feet of a building corner must be treated.
    - 3. All glass railings must be treated.
    - 4. All glass on enclosed building connections shall be treated up to sixty (60) feet above-grade.
  - (b) <u>Sky-bridges</u>. For buildings and structures of any size, all glass on above-ground bridges must be treated.
  - (c) <u>At-grade glass</u>. For buildings and structures of any size, all at-grade glass features such as sound walls or glass screens must be treated.



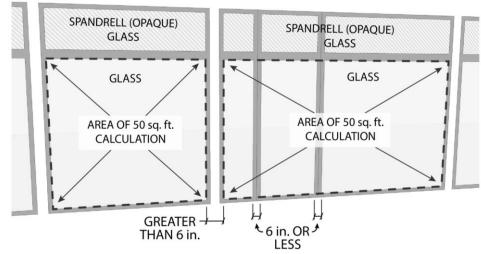
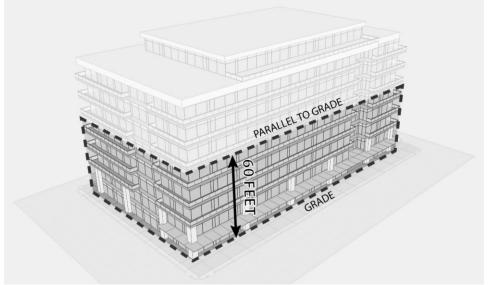
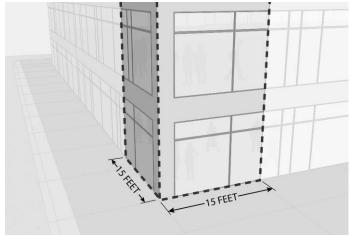


Figure 1











# **ELECTRIC VEHICLE CHARGING**

EV Ready / EV Installed Requirements for Parking Facilities

NEW ORDINANCE Effective 1/20/2021

#### 28.141(8)(e)

#### **ELECTRIC VEHICLE CHARGING STATION REQUIREMENTS.**

Parking facilities shall be designed and built to meet the following requirements:

#### 28.141(8)(e)(1) APPLICABILITY.

- Any new parking facility
- Any parking facility expanded by 50%, as measured in parking spaces being created after January 1, 2021.
- A parking facility may be maintained or reconstructed without triggering the requirements of this subdivision; however, where more than 10,000 square feet of the paving and base in place on January 1, 2021 is removed from an existing parking lot and new paving and base is installed, these requirements shall apply.

RESERVED RESERVED FLATION

#### 28.141(8)(e)(2)

#### RESIDENTIAL STANDARDS.

Where 6 or more parking spaces are being provided for residential uses, the following standards must be met:

# 28.141(8)(e)(3)

# NON-RESIDENTIAL STANDARDS.

Where parking is being provided for certain uses where people park in excess of six hours, as specified in this paragraph, the following standards must be met:

#### ELECTRIC VEHICLE CHARGING STATION REQUIREMENTS

	RESIDEN	TIAL Spaces	NON-RESIDENTIAL Spaces		
YEARS	EV Ready EV Installed		EV Ready	EV Installed	
2021-2025	10% 2%		10%	1%	
2026-2030	20% 4%		20%	2%	
2031-2035	30% 6%		30%	3%	
2036-2040	40% 8%		40%	4%	
2041+	50% 10%		50%	5%	
Application:	Where six or more parking spaces are being provided for residential uses		Where parking is being provid for certain uses where people park in excess of six hours		

#### 28.141(8)(e)(4)

#### **ACCESSIBLE STATIONS.**

Accessible charging stations shall be provided based on the following:

#### 28.141(8)(e)(5)

#### **COMPUTATION.**

Fractional space requirements of up to one-half ( $\frac{1}{2}$ ) space shall be rounded down to the next whole number and greater than one-half ( $\frac{1}{2}$ ) rounded up to the next whole number.

#### ACCESSIBLE STATIONS

Number of EV Installed Spaces Required	Minimum Accessible EV Installed Spaces
0-2	0
3-50	1
51-100	2
101+	3 +1 for each additional 50 spaces

#### 28.141(8)(e)(3)(a)

#### Applies to these uses:

- College, University or Similar Institutions of Higher Learning
- ii. Hotel, Inn, Motel
- iii. Hospital and Health Clinic
- iv. Office
- v. Parking Facility, Private and Public
- vi. Schools, Arts, Technical or Trade
- vii. Any other use where people are typically expected to park vehicles in excess of six hours except for those uses specified as not applicable

#### 28.141(8)(e)(3)(b)

# Does <u>Not</u> Apply to these uses:

- i. Manufacturing
- ii. Restaurants
- iii. Retail
- iv. Service Business
- v. Warehousing and Storage



#### **EXAMPLE CALCULATIONS** using the first tier 2021-2025 requirements:

#### 28.141(8)(e)(2)

#### **RESIDENTIAL STANDARDS.**

Where 6 or more parking spaces are being provided for residential uses, the following standards must be met:

#### 28.141(8)(e)(3)

#### **NON-RESIDENTIAL STANDARDS.**

Where parking is being provided for certain uses where people park in excess of six hours, as specified in this paragraph, the following standards must be met:

Total Stalls in Lot         6 Stalls         26 Stalls         50 Stalls         75 Stalls         100 Stalls         200 Stalls           EV Ready 10% =         (0.6) = 1 space         (2.6) = 3 spaces         (5) = 5 spaces         (7.5) = 7 spaces         (10) = 10 spaces         (20) = 20 stalls	RESIDENTIAL PARKING STRUCTURES AND LOTS							
<b>EV Ready 10% =</b> (0.6) = 1 space (2.6) = 3 spaces (5) = 5 spaces (7.5) = 7 spaces (10) = 10 spaces (20) = 20 s	Total Stalls in Lot6 Stalls26 Stalls50 Stalls75 Stalls100 Stalls200 Stalls							
	EV Ready 10% =	(0.6) = 1 space	(2.6) = 3 spaces	(5) = 5 spaces	(7.5) = 7 spaces	(10) = 10 spaces	(20) = 20 spaces	
<b>EV Installed 2%* =</b> (0.12) = 0 spaces (0.52) = 1 space (1) = 1 space (1.5) = 1 space (2) = 2 spaces (4) = 4 space	EV Installed 2%* =	(0.12) = 0 spaces	(0.52) = 1 space	(1) = 1 space	(1.5) = 1 space	(2) = 2 spaces	(4) = 4 spaces	

\*25 total stalls or less = No EV-Installed spaces required

NOTE: Fractional space requirements of up to one-half space shall be rounded down to the next whole number; >1/2 rounds up to next whole number

NON-RESIDENTIAL PARKING STRUCTURES AND LOTS							
Total Stalls in Lot	6 Stalls	26 Stalls	50 Stalls	75 Stalls	100 Stalls	200 Stalls	
EV Ready 10% =	(0.6) = 1 space	(2.6) = 3 spaces	(5) = 5 spaces	(7.5) = 7 spaces	(10) = 10 spaces	(20) = 20 spaces	
EV Installed 1%* =	(0.06) = 0 spaces	(0.26) = 0 spaces	(0.5) = 0 spaces	(0.75) = 1 space	(1) = 1 space	(2) = 2 stalls	

\*50 total stalls or less = No EV-Installed spaces required

NOTE: Fractional space requirements of up to one-half space shall be rounded down to the next whole number; >1/2 rounds up to next whole number

JuiceBox

# 28.211 **DEFINITIONS.**

**ELECTRIC VEHICLE** or **EV** means any motor vehicle that is licensed and registered for operation on public and private highways, roads, and streets, and that operates either partially or exclusively, on electrical energy from the grid, or an off-board source, that is stored onboard via a battery for motive purpose. Electric Vehicle does not include an electric bicycle. Electric Vehicle includes:

(1) A battery electric vehicle; and

(2) A plug-in hybrid electric vehicle.

#### **ELECTRIC VEHICLE CHARGING**

**STATION**. A parking space, or parking spaces, that is, or are, equipped with and served by electric vehicle supply equipment for the purpose of transferring electric energy to a battery or other energy storage device in an electric vehicle. The terms Level I, 2, and 3 charging stations are the most common electric vehicle charging levels, and include the following specifications:

 (1) Level I is considered slow charging and operates on a 15 to 20 amp breaker on a 120 volt AC circuit.
 (2) Level 2 is considered medium charging and operates on a 40 to 100 amp breaker on a 208 or 240 volt AC circuit. (3) Level 3 is considered "fast" or "rapid" charging and typically operates on a 60 amp or higher breaker on a 480 volt or higher three phase

circuit with special grounding equipment. Level 3 stations are primarily for commercial and public applications and are typically characterized by industrial grade electrical outlets that allow for faster recharging of electric vehicles.

#### ELECTRIC VEHICLE SUPPLY EQUIPMENT

or **EVSE** are the conductors, including the ungrounded, grounded, and equipment grounding conductors, and the electric vehicle connectors, attachment plugs, and all other fittings, devices, power outlets, or apparatus installed specifically for the purpose of transferring energy between the premises wiring and the electric vehicle.

**EV READY SPACE** means a designated parking space which is provided with electrical panel capacity and space to support a minimum 40-ampere, 208/240-volt branch circuit, and the installation of raceways, both underground and surface mounted, to support the future installation of EVSE to serve the parking space.

**EV INSTALLED SPACE** means a parking space that is served by an electric vehicle charging station, at a minimum of Level 2.